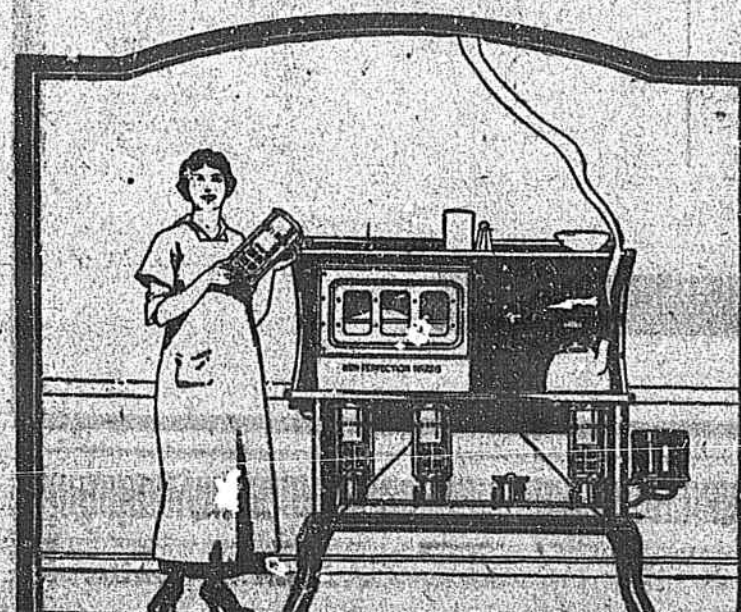


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INFANTS CHILDREN
Promotes Digestion, Liver
Action and Rest. Contains neither
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Laxative
Action
No
Pain
No
Discomfort
No
Danger
A Perfect Remedy for Constipation,
Sour Stomach, Biliousness,
Wind, Feverishness and
LOSS OF SLEEP.
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A New Perfection Oil Cook Stove, the stove with the long blue chimney, gives kitchen comfort in 2,000,000 American homes.

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New Perfection Oil Cook Stoves are made in many styles and sizes. They are sold by most good dealers.

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IT'S THE LONG BLUE CHIMNEY

BEST PAID MEN ARE NOW ASKING FOR HIGHER PAY

Government Officials Fail To Find Any
Justice in Demands Of Train Ser-
vice Employees.

By Judson C. Welliver in The Wash-
ington Times.

Administrative and legislative au-
thorities in Washington are taking a
distinctly different view of the pres-
ent effort of railway trainmen to
compel an advancement in their
wages, from any that has been taken
on former occasions.

It is very apparent that the case
for the employees seeking high-
wages is viewed with less amiability
than ordinarily. In legislative cir-
cles there has recently been serious
talk of legislation to prohibit strikes
by employees of interstate carriers,
and to provide a procedure for com-
pulsory arbitration.

The impression has gained a good
deal of ground, that certain favored
classes of employees have for a long
time been systematically aggregating
to themselves most of the increases
in wages.

Highest Paid Class Of Men

On behalf of the engineers and
trainmen who are asking the de-
mand for a large wage increase, it is
urged that the higher cost of living
justifies their demand. Yet these
men, the engineers, firemen, conduc-
tors and brakemen, are the highest
paid classes of railroad labor. The
question being asked why an engineer
getting an average wage in 1913 of
\$5.20, should require an increase,
while trackmen, who were getting
\$1.58 per day, should be left out?

Again, the average wage of conduc-
tors in 1913 is shown by the statistics
of the Interstate Commerce Commis-
sion to have been \$4.30 per day. At
the same time, telegraph operators
and dispatchers were getting an aver-
age wage of \$2.52 per day.

If the increased cost of living for
the \$4.30 conductor necessitates a
large increase in his compensation,
where does the \$2.52 dispatcher come
in?

No demand is being urged on be-
half of the operators and dispatchers,
and some of the railroads have lately
been intimating vigorously that if a
big additional burden must be laid
on their labor funds they would like
to give the benefit to the poorer paid
classes of employees.

The truth of the whole business is
that, as a whole, the railroad em-
ployees of the country are not very
highly paid as compared to other peo-
ple. A few classes of railroad men
are paid very high wages. The most
fortunate of all these classes are the
engineers, firemen, conductors and
brakemen.

Rates Rising Rapidly

Not only are these four classes paid
much more liberally than other em-
ployees, but the figures show that
their rate of wages has been rising
more rapidly than that of any other
classes.

In 1914 the Interstate Commerce
Commission report showed the num-
ber of railroad employees for the en-
tire country to be 1,710,296. Out of
this number there were 62,021 en-
gineers, 64,959 firemen, 48,201 con-
ductors, and 136,809 other trainmen;
a total of 311,890, or just about one-
sixth of the entire number.

At that same date, the number of
trackmen, exclusive of firemen, was
337,451. That is, the number of com-
mon laborers on the section was
greater than the entire roll of en-
gineers, firemen, conductors, and
brakemen. Yet this huge army of
trackmen was working for an aver-
age wage of \$1.58 per day, while en-
gineers were getting \$5.20, firemen
were getting \$2.15, conductors were
getting \$4.30, and other trainmen
were getting \$3.04. These figures are
the commission's averages for the en-
tire country.

Condition Bettered

Largely because they are the best
organized class of railway workers
and have been unremitting in their
demands for better wages, these four
classes have succeeded in bettering
their condition rapidly and regularly,
at the expense of the other classes,
which are not so highly organized.

The trainmen, wherever they insist
on a wage increase, have on their
side, the tremendously potent argu-
ment that if they don't get what they
ask, they can walk out and tie up the
whole railroad system. No other
class of employees could do this, be-
cause no other is so instantly indis-
pensable.

How effectively the four favored
classes have used their power is
shown by the fact that, in the
last year from 1914 to 1915, the
salaries of general officers in-
creased an average of 17 per cent.
The same increase was made in the
salaries of assistant secretaries.

These same ten years the
wage of general office clerks in-
creased 18 per cent, while the wages of
firemen increased 20 per cent.

During those same ten years the
wages of telegraph operators and dis-
patchers increased 10 per cent, while
the wages of trackmen increased 10 per cent.

Impressive Statement

Here is an impressive statement of
fact about railway wages that ought
not to escape attention. There were
a total of 37,873 employees classified
as switch tenders, crossing tenders,
and watchmen. These were receiv-
ing in 1912 an average of \$2.70 per
day, which was actually a cents a
day less than they had been receiv-
ing ten years earlier.

At that time there were 48,201 con-
ductors with whom the statistics
dealt. The conductors, therefore,
were only a slightly more nume-
rous class than the tenders and watch-
men; yet, while the conductors had
had their wages raised from \$3.38 to
\$4.29 per day, the less fortunate
class of tenders and watchmen had
to stand a reduction from \$1.76 to
\$1.70 per day.

If the cost of living has been steady-
ly advancing for conductors, so as to
justify an increase of 27 per cent in
their wages, it seems difficult to ex-
plain why that same cost of living
should have fallen sufficiently to war-
rant a decrease of 3 per cent in the
wages of switch tenders, crossing
tenders, and watchmen.

Take the single classification of
general office clerks. There were
\$7,106 of these according to the of-
ficial report. A much larger number
than of either engineers, firemen, or
conductors.

These general office clerks were
paid an average of \$2.21 per day in
1903, and of \$2.50 in 1912; an in-
crease of only 13 per cent in the ten-
year period.

General office clerks, without ex-
ception, are compelled to live in
cities, where cost of living is high.

Engineers Better Off

Engineers, on the other hand, are
distributed between large towns and
small towns; on the average, their
living circumstances ought to make
their expenses average considerably
less than those of office clerks, yet
the statistics show that engineers
have received in the ten-year period
an increase of 24 per cent in their
wages, making them average exactly
\$5 per day, while general office
clerks have received an increase of
only 13 per cent, making them aver-
age \$2.50.

One of the worst underpaid classi-
fications of railway employees is that
of the station agents. There are just
about 4,000 of these in the country,
or nearly as many as the number of
conductors.

In 1903 station agents averaged
\$1.80 a day, and in 1912 they had
been raised to only \$2.20 a day, while
in that same time conductors had
advanced from \$3.38 to \$4.29.

That is, the station agent in 1912
was getting just about half the wages
of the conductor, and in ten years he
had had an average increase of 17
per cent, while the conductor's in-
crease had been 27 per cent.

Here are two of the most nume-
rous classes of railway employees:
Trainmen, other than engineers,
firemen, and conductors, numbered
136,809, while trackmen numbered
337,451. The statistics show that the
trainmen were getting in 1903 an
average of \$1.41 per day, and in 1912
an average of \$1.69 per day, an in-
crease of 14 per cent.

What Others Get

On the other hand, the classifica-
tion of other trainmen was getting in
1903 \$2.17 per day, and in 1912, \$2.96
per day, an increase of 36 per cent.
In percentage, this is the largest ad-
vance received by any single class of
railway employees during this decade.

A general survey of wage condi-
tions in the railway service and in
other industries, it is believed, would
show that in the last fifteen years
the highly organized and favored
classes of railway wage earners have
had their incomes increased more
than almost any other class of work-
ers in the country, while the much
more numerous, but less effectively
organized classes of railway workers
have probably received rather less
increases than other industrial work-
ers in general.

In view of the strong feeling that
these most fortunate classes of the
railway employees are now making
excessive and unreasonable demands,
attention is now being called as
never before to these general dis-
crepancies. There is a strong dis-
position to inaugurate a general and
sweeping investigation of the whole
question of railway wages with a
view to establishing some sort of pub-
lic regulation not unlike that already
applied to railroad rates, in the in-
terest of employees and public alike.
Meanwhile, there is a marked dis-
position to extend further favors to
those classes already most highly
favored, at the expense of other
classes of employees who appear to be
getting very low wages.

Four national railroad unions
threaten to tie up every steam
road in the country. The num-
ber of employees involved may
be 100,000. They seem to
have it in their power to stop
all freight and passenger traf-
fic on 250,000 miles of track.
We have never had a strike
of such scope and magnitude
in the history of American
industry and commerce and a
stoppage of 100,000 people would
stop the life of the nation.
The railway industry
ought to be a source of wages
and better living conditions
for the people.

The Victor.

With joyous shouts, high in the air
Our hats for him who wins are tossed,
But what of him, of honor bare,
Who strove as honestly—and lost?

Even though his eyes with tears are dim,
All thought of him will quickly fade
Except when men refer to him
For the poor showing that he made.

And yet we sometimes must confess,
As we see men to honor rise,
Some are less worthy of success
Than he who failed to win the prize.

Because he was unfairly used
By those we thought should him be-
friend
And that his good name was abused
By some who to such tricks descend.

The world in this seems to delight
Nor of the means employed complains
No matter if by wrong or right
Of one who victory attains.

But we in this may solace find,
View it from any point we choose—
It seems as if it was designed
For some to win and some to lose.
—Boston Globe.

FOURTH FALL OF LUTZK

Fortress Taken by Russians Has Seen
Much Fighting in Present War.

Lutzk, captured by the Russians in
their latest offensive against Austria,
is situated thirty-five miles from the
Galician border and is a modern
stronghold erected within recent years
by Russia to ward off possible Aus-
trian aggression. It is the eastern an-
gle of the Volhynian fortress, the two
others, Dubno, the apex, situated thirty
miles to the southeast, still in Teu-
ton hands, and Rovno, thirty miles to
the east, having been built for its sup-
port. When the German and Austro-
Hungarian armies last autumn com-
pelled the Russians to retire on the
whole 500 mile front Lutzk was one of
the most bitterly contested points on
the southern part of the line. It changed
hands three times, falling last into
Teuton hands on Oct. 1.

The fortress lies halfway between
Rovno and Kovel, on the important
railway line that runs from Brest
Litovsk to the region southwest of
Kiev. It is this railway sector, be-
tween Rovno and Kovel, that has been
the objective of the Russian attacks
ever since the Teuton offensive came to
a standstill eight months ago, for its
control would give the Russians a free
hand to operate southward against the
lines in Galicia.

Without holding Lutzk the Austrians
will have a hard task maintaining
themselves in the smaller Volhynian
fortress to the southeast, Dubno, and
once Dubno has fallen the road is open
to the czar's forces for another inva-
sion of western Galicia. Such an inva-
sion, if accompanied by simultaneous
attacks against the Bukovina lines,
would present a formidable menace to
the whole southernmost Austro-Hun-
garian front, as it would threaten it
with being cut off or driven to the
north into the hands of the army in
rading Galia.

The town and district of Lutzk a
little commercial importance. Lutzk
was at one time a thriving center, with
a population of about 40,000, but since
the Polish wars in the last half of the
sixteenth century it has steadily de-
clined.

"SALLY ON CAR," SALONIKI.

Tommyes Adopt Various Methods to
Keep Home Folks Posted.

The Rev. F. H. Gillingham, the Essex
cricketer, who has been censoring sol-
diers' letters at the front, has been
telling some of his experiences. The
main object of most Tommyes seems
to be to let their relatives know where
they are, and all sorts of schemes
have been discovered. A common one
at the start of the war was to place
dots under certain letters which, when
read together, gave information as to
the writer's whereabouts, but parents
soon began to receive letters with a
confused jumbling of dots placed un-
der other letters by the censor.

The story goes, however, that a cen-
sor was not wide awake enough to see
through one little sentence in a letter
from a member of the Mediterranean
expeditionary force. It was in the form
of a postscript and read: "I met Sally
on the car." Whether one sees it or
not depends on how he pronounces
Saloniki.

HAS NEW CHINESE ALPHABET

Dr. Lam Hopes to Revolutionize His
Country's Written Language.

Dr. T. P. Lam, a distinguished Chi-
nese scholar, left San Francisco for
China, where he will offer his fellow
countrymen an alphabet which if
adopted, he said, would revolutionize
the written language of China.

After laboring for six years while
occupying the chair of associate profes-
sor of oriental languages and literature
in the University of London Dr. Lam
has completed an alphabet of fifty-six
characters. In addition to the new al-
phabet, Dr. Lam has devised a tele-
graphic code of dots and dashes to be
used in connection therewith. He ex-
pects, he said, to have proved practical
in a test given in the Chinese schools.
He was led to devise the alphabet, he
said, because Chinese people's written
language was so cumbersome that it
proved a handicap in a fast-moving
world.

Duties of Vice President

The duties of the vice president of
the United States were defined by an
opinion in a constitutional case in
February, 1915. The vice president
was held to be a member of the cabinet
and was held to be a member of the
executive branch of the government.

YOUR WAGON IS READY FOR YOU

I have just rolled out a carload of

GOOD OLD MITCHELL WAGONS

Known as the "Monarch of the Road," a wagon with a
record, having been manufactured and giving satisfaction
continuously since 1834, or for 82 years.

All sizes and every size
at the right price

And if you're thinking of buying a Buggy I am equally as
well prepared to serve you, Brockway, Hackney, Wash-
ington, Delker Bros., and Kentucky.

Prices and Terms to suit

Fred G. Tribble

Successor to Davis Bros.

West Benson St.

Complete stock of Harness, Laprobes, Whips, etc.

Just Arrived

A fresh stock of Johnson's Prepared Wax and
Cleaner. The Perfect Polish for Floors, Pian-
os, Furniture and

Automobiles

Prevents the gathering of dust and sheds
water. Try it.

Sullivan Hardware Company

Anderson, S. C. Belton, S. C. Greenville, S. C.

Hot Water Each Morning Puts Roses in Your Cheeks



To look one's best and feel one's
best is to enjoy an inside bath each
morning to flush from the system the
previous day's waste, sour fermenta-
tions and poisons which are then suck-
ed into the blood. Just as coal, when it
burns, leaves behind a certain amount of
incombustible material in the form of ash
and soot, so the food and drink taken each
day leaves in the alimentary organs a cer-
tain amount of indigestible material,
which, if not eliminated, forms tox-
ins and poisons which are then suck-
ed into the blood through the very
absorbents which are intended to suck
only nutriment to sustain the body.

If you want to see the glow of
healthy blood in your cheeks, to see
your skin get clearer and clearer,
you are told to drink every morning
upon arising, a glass of hot water
with a teaspoonful of Epsom's
sulfate in it which is harmless
means of washing the waste material
and toxins from the system, liver
function and bowels thus clearing
away and purifying the entire
system.

Women who desire to eliminate
toxins from their complexion should
just as soon use a skin wash
as a purifying agent.

The intelligent use of Epsom's
sulfate is the best and most effective
method of flushing the system and
purifying the blood.